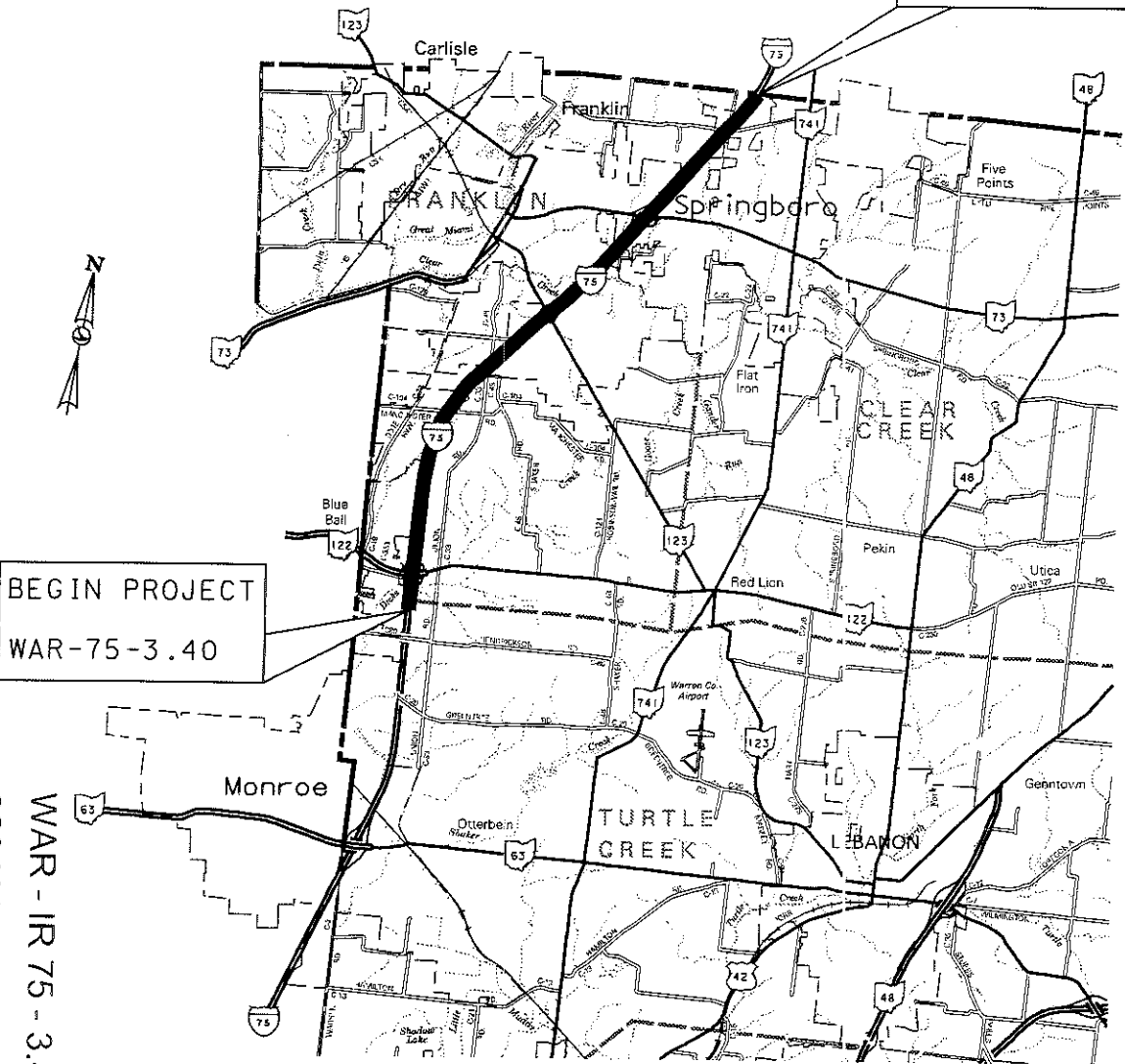


OHIO DEPARTMENT OF TRANSPORTATION

LOCATION MAP
WARREN COUNTY

END PROJECT
WAR-75-12.20



BEGIN PROJECT
WAR-75-3.40

WAR - IR 75 - 3.40 (Cities of Middletown
020406 PID - 21773
Dist 8 9/11/2002

PROJECT DESCRIPTION

PLANING AND PAVING THE EXISTING MAINLINE PAVEMENT AND SHOULDERS.
PLANING AND PAVING THE ACCELERATION, DECELERATION LANES AND RAMPS.
INSTALLING PAVEMENT MARKINGS, RAISED PAVEMENT MARKERS, LOOP DETECTORS
AND RUMBLE STRIPS.

PART	COUNTY	ROUTE	SECTIONS	PROJECT TERMINII		NET LENGTH MILES	CITY	TOWNSHIP
				BEGIN	END			
1	WAR	I-75	3.60	3.40	12.20	8.80	MIDDLETOWN	
			4.96-6.58					
			6.86				FRANKLIN	
			6.98-9.18					
			9.70-10.41					
			11.35				SPRINGBORO	
			11.63					

INDEX OF SHEETS:

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RAISED PAVEMENT MARKER	11
DROPOFFS IN WORK ZONES	12
RAMP LOCATIONS	13-15
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1997 SPECIFICATIONS

The standard specifications of the State of Ohio, Department of Transportation, including changes and supplemental specifications listed in the proposal shall govern this improvement.

I hereby approve these plans and declare that the making of this improvement will not require the closing to traffic of the highway and that provisions for the maintenance and safety will be as set forth on plans and estimates.

ENGINEERS SEAL:



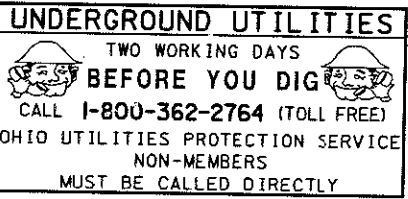
SIGNED: *[Signature]*
DATE: 5-22-02

STANDARD DRAWINGS

BP-3.1	07-28-00	MT-105.10M	04-25-94
BP-6.1	07-28-00	MT-105.11M	04-25-94
BP-9.1	07-28-00	TC-41.20	01-19-01
MT-35.10	04-20-01	TC-65.10	10-19-01
MT-95.30	04-19-02	TC-65.11	10-19-01
MT-98.12	04-19-02	TC-65.12	10-19-01
MT-98.13	04-19-02	TC-71.10	04-19-02
MT-98.14	04-19-02	TC-72.20	01-19-01
MT-98.15	04-19-02		
MT-98.16	04-19-02		
MT-98.17M	04-25-94		
MT-98.18M	04-25-94		
MT-98.19M	03-01-96		
MT-99.20M	01-30-95		

SUPPLEMENTAL
SPECIFICATIONS

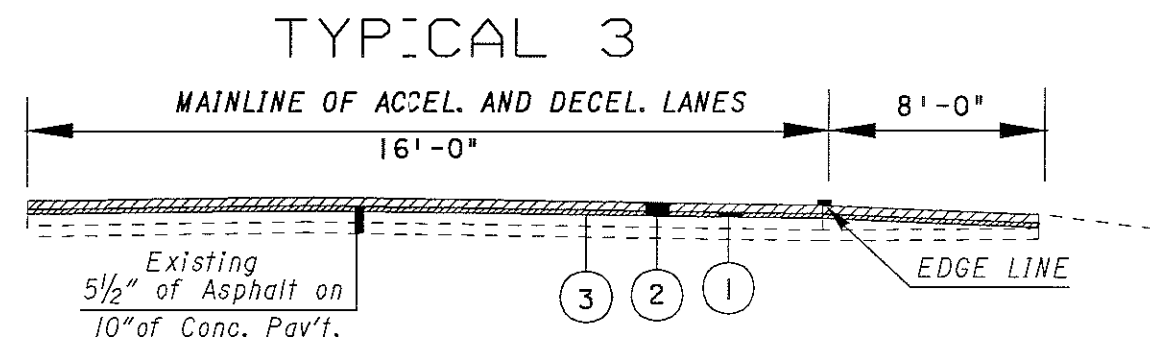
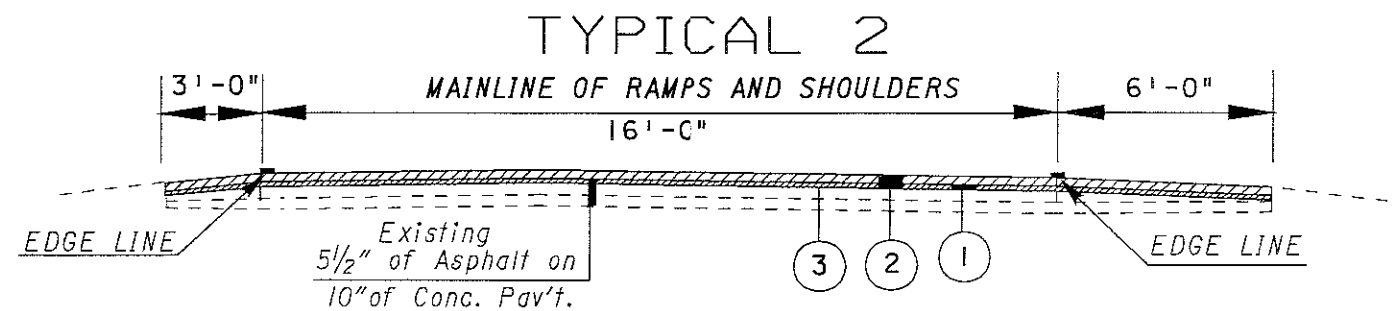
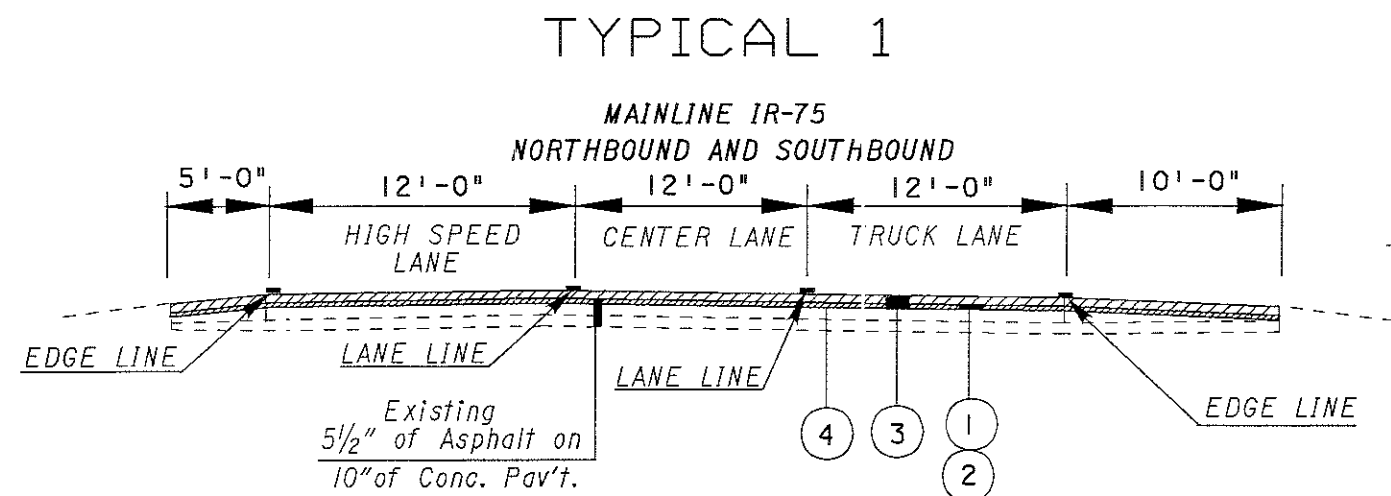
SS858	11-17-00
SS906	05-05-98
SS908	11-07-00
SS1082	01-11-00



PLAN PREPARED BY:
DISTRICT 8
PRODUCTION

Approved: *[Signature]*
Date: 5/22/02 District Deputy Director of Transportation

Approved: *[Signature]*
Date: 6-5-02 Director, Department of Transportation



LEGEND

- ① ITEM 254 PAVEMENT PLANING, BITUMINOUS, 1/2 IN. DEPTH
- * ② ITEM 254 PAVEMENT PLANING, BITUMINOUS, 1 1/2 IN. DEPTH (UNDER OVERHEAD BRIDGES)
- ③ ITEM 858 SURFACE COURSE, 12.5MM, TYPE A (446) , 1/2 IN. THICKNESS
- ④ ITEM 407 TACK COAT

MAINLINE, RAMPS, DECELERATION LANES AND ACCELERATION LANES INCLUDE SHOULDERS

QUANTITIES CARRIED TO THE GENERAL SUMMARY		PAVEMENT DATA											
NOTE: DO NOT DISTURB THE EXISTING LANDSCAPE AREAS WITHIN THE PROJECT LIMITS. DESCRIPTION		LENGTH		WP FEET AVG.	TYPICAL	EXISTING TYPE PAVEMENT	PAVEMENT AREA SQ. YDS.	407	858		DEPTH IN.	254	
		MILES	LIN. FT.					TACK COAT	ASPHALT CONCRETE SURFACE COURSE 12.5 MM TYPE A (446)			PAVEMENT PLANING BITUMINOUS SQ. YD.	PATCHING PLANED SURFACE SQ. YD.
								@ 0.075 GAL./S.Y. GAL.	THICK INCHES	CU. YD.			
N.B. AND S.B. IR-75 - 3.40-6.30 MAINLINE		2.90	15312	102	I	ASPHALT	173536	13015	1 1/2	7231	1/2	173536	8677
*N.B. AND S.B. IR-75 - 6.30-6.38 MAINLINE		.08	422	102	2	ASPHALT	4783	359	1 1/2	199	1 1/2	4783	239
N.B. AND S.B. IR-75 - 6.38-7.18 MAINLINE		.80	4224	102	I	ASPHALT	47872	3590	1 1/2	1995	1/2	47872	2394
*N.B. AND S.B. IR-75 - 7.18-7.26 MAINLINE		.08	422	102	2	ASPHALT	4783	359	1 1/2	199	1 1/2	4783	239
N.B. AND S.B. IR-75 - 7.26-9.14 MAINLINE		1.88	9926	102	I	ASPHALT	112495	8437	1 1/2	4687	1/2	112495	5625
*N.B. AND S.B. IR-75 - 9.14-9.22 MAINLINE		.08	422	102	2	ASPHALT	4783	359	1 1/2	199	1 1/2	4783	239
N.B. AND S.B. IR-75 - 9.22-11.42 MAINLINE		2.20	11616	102	I	ASPHALT	131648	9874	1 1/2	5485	1/2	131648	6582
*N.B. AND S.B. IR-75 - 11.42-11.50 MAINLINE		.08	422	102	2	ASPHALT	4783	359	1 1/2	199	1 1/2	4783	239
N.B. AND S.B. IR-75 - 11.50-12.20 MAINLINE		.70	3696	102	I	ASPHALT	14888	1117	1 1/2	620	1/2	14888	744
DEDUCTIONS FOR BRIDGES (SEE SHEET 4)													
TOTALS THIS SHEET		8.80	46462				499571	37469		20814		499571	24978

ASPHALT CONCRETE

WAR-75-3.40

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QUANTITIES CARRIED TO THE GENERAL SUMMARY

PAVEMENT DATA

DESCRIPTION (QUANTITIES INCLUDES SHOULDERS)	LENGTH		WP FEET AVG.	T Y P I C A L	EXISTING TYPE PAVEMENT	PAVEMENT AREA	407	858		254		
	MILES	LIN. FT.					BACK COAT @ 0.075	ASPHALT CONCRETE SURFACE COURSE 12.5 MM TYPE A (446)		DEPTH IN.	PAVEMENT PLANING BITUMINOUS SQ. YD.	PATCHING PLANED SURFACE SQ. YD.
						SQ. YDS.	GAL./S.Y. GAL.	THICK INCHES	CU. YD.			
RAMP "A" @ SR-122		158	25	2	ASPHALT	439	33	1.50	18	1/2	439	22
RAMP "B" @ SR-122		675	25	2	ASPHALT	1875	141	1.50	78	1/2	1875	94
RAMPS "C" AND "G" @ SR-122		1290	25	2	ASPHALT	3583	269	1.50	149	1/2	3583	179
RAMPS "D" AND "H" @ SR-122		1395	25	2	ASPHALT	3875	291	1.50	161	1/2	3875	194
RAMP "E" @ SR-122		1282	25	2	ASPHALT	3561	267	1.50	148	1/2	3561	178
AUXILARY LANE @ SR-122 BETWEEN RAMPS C & D		530	25	3	ASPHALT	1472	110	1.50	61	1/2	1472	74
AUXILARY LANE @ SR-122 BETWEEN RAMPS C & D		770	40	3	ASPHALT	3422	257	1.50	143	1/2	3422	171
AUXILARY LANE @ SR-122 BETWEEN RAMPS C & D		280	25	3	ASPHALT	778	58	1.50	32	1/2	778	39
N.B. DECELERATION LANE TO RAMP C @ SR-122		645	16	3	ASPHALT	1147	86	1.50	48	1/2	1147	57
N.B. DECELERATION LANE TO RAMP C @ SR-122		570	12.5	3	ASPHALT	792	59	1.50	33	1/2	792	40
N.B. ACCELERATION FROM RAMP D @ SR-122		390	27.5	3	ASPHALT	1192	89	1.50	50	1/2	1192	60
N.B. ACCELERATION FROM RAMP D @ SR-122		820	12.5	3	ASPHALT	1139	85	1.50	47	1/2	1139	57
S.B. DECELERATION LANE TO RAMP B @ SR-122		830	16	3	ASPHALT	1476	111	1.50	62	1/2	1476	74
S.B. DECELERATION LANE TO RAMP B @ SR-122		277	26	3	ASPHALT	800	60	1.50	33	1/2	800	40
S.B. ACCELERATION FROM RAMP A @ SR-122		571	28	3	ASPHALT	1776	133	1.50	74	1/2	1776	89
S.B. ACCELERATION FROM RAMP A @ SR-122		990	10	3	ASPHALT	1100	83	1.50	46	1/2	1100	55
RAMP "M" @ SR-123		1215	25	2	ASPHALT	3375	253	1.50	141	1/2	3375	169
RAMP "M" @ SR-123		360	38	2	ASPHALT	1520	114	1.50	63	1/2	1520	76
RAMP "L" @ SR-123		675	25	2	ASPHALT	1875	141	1.50	78	1/2	1875	94
RAMP "K" @ SR-123		730	25	2	ASPHALT	2028	152	1.50	85	1/2	2028	101
RAMP "K" @ SR-123		275	37	2	ASPHALT	1131	85	1.50	47	1/2	1131	57
RAMP "J" @ SR-123		1970	25	2	ASPHALT	5472	410	1.50	228	1/2	5472	274
N.B. DECELERATION LANE TO RAMP M @ SR-123		970	16	3	ASPHALT	1724	129	1.50	72	1/2	1724	86
N.B. DECELERATION LANE TO RAMP M @ SR-123		285	26	3	ASPHALT	823	62	1.50	34	1/2	823	41
N.B. ACCELERATION FROM RAMP L @ SR-123		685	23	3	ASPHALT	1751	131	1.50	73	1/2	1751	88
N.B. ACCELERATION FROM RAMP L @ SR-123		680	10	3	ASPHALT	756	57	1.50	32	1/2	756	38
S.B. DECELERATION LANE TO RAMP K @ SR-123		940	16	3	ASPHALT	1671	125	1.50	70	1/2	1671	84
S.B. DECELERATION LANE TO RAMP K @ SR-123		310	26	3	ASPHALT	896	67	1.50	37	1/2	896	45
S.B. ACCELERATION FROM RAMP J @ SR-123		680	23	3	ASPHALT	1738	130	1.50	72	1/2	1738	87
S.B. ACCELERATION FROM RAMP J @ SR-123		975	10	3	ASPHALT	1083	81	1.50	45	1/2	1083	54
RAMP "A" @ SR-73		1350	25	3	ASPHALT	3750	281	1.50	156	1/2	3750	188
RAMP "B" @ SR-73		420	37	2	ASPHALT	1727	130	1.50	72	1/2	1727	86
RAMP "B" @ SR-73		1650	25	2	ASPHALT	4583	344	1.50	191	1/2	4583	229
RAMP "C" @ SR-73		835	25	2	ASPHALT	2319	174	1.50	97	1/2	2319	116
RAMP "D" @ SR-73		565	37	2	ASPHALT	2323	174	1.50	97	1/2	2323	116
RAMP "D" @ SR-73		520	25	2	ASPHALT	1444	108	1.50	60	1/2	1444	72
N.B. DECELERATION LANE TO RAMP D @ SR-73		1545	16	3	ASPHALT	2747	206	1.50	114	1/2	2747	137
N.B. DECELERATION LANE TO RAMP D @ SR-73		255	26	3	ASPHALT	737	55	1.50	31	1/2	737	37
N.B. ACCELERATION FROM RAMP C @ SR-73		600	28	3	ASPHALT	1867	140	1.50	78	1/2	1867	94
N.B. ACCELERATION FROM RAMP C @ SR-73		1065	10	3	ASPHALT	1183	89	1.50	49	1/2	1183	59
S.B. DECELERATION LANE TO RAMP B @ SR-73		1505	16	3	ASPHALT	2676	201	1.50	112	1/2	2676	134
S.B. DECELERATION LANE TO RAMP B @ SR-73		270	28	3	ASPHALT	840	63	1.50	35	1/2	840	42
S.B. ACCELERATION FROM RAMP A @ SR-73		602	28	3	ASPHALT	1873	141	1.50	78	1/2	1873	94
S.B. ACCELERATION FROM RAMP A @ SR-73		890	10	3	ASPHALT	989	74	1.50	41	1/2	989	49
TOTALS THIS SHEET						83328	6249		3471		83328	4170

ASPHALT CONCRETE

WAR-75-3.40

3
16

EXTRA AREAS AND DEDUCTIONS

WAR-75-3.40

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CONTINGENCY QUANTITIES

THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK LISTED IN THE GENERAL SUMMARY FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED "AS DIRECTED BY THE ENGINEER" UNLESS AUTHORIZED BY THE ENGINEER. THE ACTUAL WORK LOCATIONS AND QUANTITIES USED AT THE ENGINEER'S DIRECTION SHALL BE MADE A MATTER OF RECORD BY INCORPORATION INTO THE FINAL CHANGE ORDER GOVERNING COMPLETION OF THE PROJECT.

R.A.P. IDENTIFICATION FOR 441 ASPHALT DESIGNS

FOR THE PURPOSES OF ASPHALT DESIGNS UTILIZING RECYCLED ASPHALT PAVEMENT, (R.A.P.), THE EXISTING ASPHALT WAS PLACED UNDER PROJECT 343 (97). THE CONTRACTOR SHALL FURNISH THIS INFORMATION WITH THE REQUIRED DESIGN PACKET TO THE BITUMINOUS CONCRETE SECTION IN COLUMBUS.

ITEM 407-TACK COAT

THE RATE OF APPLICATION OF 407 TACK COAT SHALL BE SUBJECT TO ADJUSTMENT, AS DIRECTED BY THE ENGINEER. FOR ESTIMATING PURPOSES ONLY, THE PLAN QUANTITIES INDICATE AN AVERAGE APPLICATION RATE OF:

0.075 GALLONS PER SQUARE YARD OF TACK COAT

ITEM 623- CONSTRUCTION LAYOUT, AS PER PLAN

PRIOR TO THE START OF ROADWAY OPERATIONS, THE CONTRACTOR SHALL REFERENCE THE LENGTH OF THE PROJECT ON BOTH SIDES OF THE ROADWAY, IN A MANNER SATISFACTORY TO THE ENGINEER. THE PAVEMENT SHALL BE REFERENCED IN 100 FEET INCREMENTS, OR IN INCREMENTS ACCEPTABLE TO THE ENGINEER, IN A SEMIPERMANENT CONDITION.

CONVERSION OF METRIC STANDARD DRAWINGS

THE METRIC STANDARD DRAWINGS REFERENCED IN THIS PLAN SHALL BE CONVERTED TO ENGLISH UNITS USING THE SI (METRIC) TO ENGLISH CONVERSION FACTORS PROVIDED IN SECTION 109.011 OF THE 1997 CONSTRUCTION AND MATERIALS SPECIFICATIONS. THE APPENDIX OF ASTM E 380 SHALL BE UTILIZED FOR ANY ADDITIONAL CONVERSION FACTORS REQUIRED. CONVERSIONS SHALL BE APPROPRIATELY PRECISE AND SHALL REFLECT STANDARD INDUSTRY ENGLISH VALUES WHERE SUITABLE.

NOTE TO CONTRACTOR

AT THE END OF THE PROJECT AT LOG POINT 12.20, THE CONTRACTOR SHALL STOP HIS PLANNING AND PAVING OPERATION 100 LIN. FT. SHORT OF THE MONTGOMERY COUNTY LINE SO AS NOT TO DISTURB THE ("AUTOMATIC TRAFFIC RECORDER") LOOP DETECTORS THAT ARE INSTALLED IN THE PAVEMENT.

AT NO TIME WILL TRAFFIC BE PERMITTED TO TRAVEL ON THE PLANED SURFACE OF THE ROADWAY.

THE CONTRACTOR SHALL BE PERMITTED TO CLOSE RAMPS C, D, H, AND G AT SR-122 AND RAMP C AT SR-73 DUE TO THESE BEING LOOP RAMPS WITH NO ROOM TO MAINTAIN TRAFFIC DURING THE PLANING AND PAVING OPERATIONS. EACH RAMP SHALL BE CLOSED FOR ONE NIGHT ONLY FROM THE HOURS OF 10:00 P.M. TO 5:30 A.M. SUBJECT TO LIQUIDATED DAMAGES IN THE AMOUNT OF \$1000.00 PER HALF HOUR THE RAMP REMAINS CLOSED TO TRAFFIC BEYOND THE SPECIFIED TIME.

THE CONTRACTOR SHALL PLACE A PORTABLE CHANGEABLE MESSAGE SIGN ON THE RAMP TO BE CLOSED A MINIMUM OF 5 DAYS PRIOR TO THE CLOSURE WITH THE MESSAGE, RAMP CLOSED "DATE"/ 10:00 P.M. TO 5:30 A.M.

UTILITY OWNERSHIP

THE FOLLOWING UTILITIES AND OWNERS ARE LOCATED WITHIN THE WORK LIMITS OF THIS PROJECT:

CINCINNATI BELL TELEPHONE 201 EAST FOURTH STREET MAIL LOCATION 103-1175 CINCINNATI, OHIO 45202 (513) 565-7043	WARREN COUNTY WATER P.O. BOX 530 406 JUSTICE DRIVE LEBANON, OHIO 45036 513-695-1646	TIME WARNER CABLE (CABLE TV) 11252 CORNELL PARK DRIVE CINCINNATI, OHIO 45242 (513) 469-5483
CINERGY-ELECTRIC 139 E. 4TH STREET ROOM 467 A CINCINNATI, OHIO 45201 513-287-1043	CINERGY-GAS 139 E. 4TH STREET ROOM 460 A CINCINNATI, OHIO 45201 513-287-2730	TEPPCO 2700 HART ROAD LEBANON, OHIO 45036 513-932-5886

GENERAL NOTES

WAR-75-3.40

ITEM 614 - PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN, AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN, ON SITE, FOR THE DURATION OF THE PROJECT. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS MAINTAINED BY THE DIRECTOR. CLASS II UNITS WITH A MINIMUM LEGIBILITY DISTANCE OF 850 FT. SHALL BE USED.

EACH SIGN SHALL BE TRAILER MOUNTED AND EQUIPED WITH A FUNCTIONAL DIMMING MECHANISM TO DIM THE SIGN DURING DARKNESS AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE FROM A LOCAL UTILITY COMPANY.

PLACEMENT, OPERATION, MAINTENANCE, AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS WILL BE OFF, FACING AWAY FROM ALL TRAFFIC AND SHALL DISPLAY ONE OR MORE HIGH INTENSITY YELLOW REFLECTIVE SHEETING SURFACES OF 9 IN. BY 15 IN. MINIMUM SIZE FACING TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT AND TO REVISE SIGN MESSAGES, IF NECESSARY.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED, BUT NORMALLY, NOT MORE THAN TWO MESSAGE PHASES SHOULD BE EMPLOYED, ALTHOUGH THREE PHASES MAY BE USED IN UNUSUAL CONDITIONS. PCMS FORMAT PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST ONCE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK. THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF 614.03. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN TWELVE (12) HOURS INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC AND THE ENTIRE COST TO CONTROL TRAFFIC ACCRUED BY THE DEPARTMENT WILL BE DEDUCTED FROM MONEYS DUE, OF TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24 HOURS PER DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

THE REQUIREMENT TO FURNISH, INSTALL, MAINTAIN AND REMOVE A PCMS UNIT ON THIS PROJECT SHALL NOT IN ANY WAY RELIEVE THE CONTRACTOR OF HIS RESPONSIBILITIES AS OUTLINED IN 104.04 OF THE CMS.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE BID PER SIGN-MONTH FOR EACH "ITEM 614 PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN" AND SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE, AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ON THIS PROJECT IT IS ESTIMATED THAT TWO SIGNS WILL BE NEEDED FOR A SIX MONTH PERIOD. THEREFORE, A QUANTITY FOR TWELVE (12) SIGN-MONTH IS CARRIED TO THE GENERAL SUMMARY FOR THE ABOVE PURPOSE.

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR

IN ADDITION TO THE REQUIREMENTS OF 614 AND "THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" (OMUTCD), ONE UNIFORMED LAW ENFORCEMENT OFFICER AND OFFICIAL PATROL CAR WITH WORKING TOP MOUNTED EMERGENCY FLASHING LIGHTS SHALL BE PROVIDED FOR CONTROLLING TRAFFIC DURING ALL HOURS THAT LANE CLOSURES ARE IN OPERATION, INCLUDING INITIAL SET-UP PERIODS AND THE TEAR DOWN PERIODS. IF THE CONTRACTOR ELECTS TO WORK IN BOTH, THE EASTBOUND AND WESTBOUND LANES CONCURRENTLY, ONE LAW ENFORCEMENT OFFICER WITH PATROL CAR SHALL BE PROVIDED FOR EACH WORK AREA.

THE LAW ENFORCEMENT OFFICERS (LEO'S) ARE CONSIDERED TO BE EMPLOYED BY THE CONTRACTOR AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THEIR ACTIONS. ALTHOUGH THEY ARE EMPLOYED BY THE CONTRACTOR, THE PROJECT ENGINEER SHALL HAVE CONTROL OVER THEIR PLACEMENT. THE OFFICIAL PATROL CAR SHALL BE A PUBLIC SAFETY VEHICLE AS REQUIRED BY THE OHIO REVISED CODE. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR THESE SERVICES WITH:

OHIO STATE HIGHWAY PATROL PHONE NO. (513) 932-4444
WARREN COUNTY OFFICE

WARREN COUNTY SHERIFF'S DEPT. PHONE NO. (513) 932-4080

THE LAW ENFORCEMENT OFFICERS WITH PATROL CARS REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR 1120 HOURS

THE HOURS PAID SHALL INCLUDE MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ITEM 62I-RAISED PAVEMENT MARKERS (HIGH PROFILE CONVENTIONAL ONLY)

MATERIALS SUPPLIED BY THE DEPARTMENT

ALL MATERIALS ARE TO BE CONTRACTOR FURNISHED, EXCEPT THAT THE DEPARTMENT SHALL SUPPLY RAISED PAVEMENT MARKERS AND RAISED PAVEMENT MARKER CASTINGS IN THE QUANTITIES SHOWN HEREIN TO THE CONTRACTOR. PAT ITEMS FOR THE DEPARTMENT SUPPLIED MATERIALS SHALL BE INDICATED AS "INSTALLATION ONLY". THE QUANTITY AND TYPE OF DEPARTMENT SUPPLIED MATERIALS ARE SHOWN ON THIS SHEET.

THE CONTRACTOR SHALL PICK UP THE DEPARTMENT SUPPLIED RAISED PAVEMENT MARKER MATERIALS AT THE:

O.P.I.
315 PHILLIPI RD.
COLUMBUS, OHIO 45895

FOR TRANSPORT TO THE WORK SITE OR TO THE CONTRACTOR'S STORAGE FACILITY. THE RECYCLED RAISED PAVEMENT MARKER (RPM) AUTHORIZATION FORM IS TO BE SIGNED BY THE DISTRICT CONSTRUCTION ENGINEER PRIOR TO PICK UP OF THE RPMS. THE CONTRACTOR SHALL NOTIFY THE DISTRICT AND/OR THE PARTIES LISTED ON THE AUTHORIZATION FORM IN WRITING AT LEAST FIVE (5) CALENDER DAYS PRIOR TO PICK UP OF THE DEPARTMENT SUPPLIED MATERIALS. THE CONTRACTOR SHALL STORE THE RPMS WITHOUT DAMAGE OR CONTAMINATION WITH FOREIGN MATTER. A DEDUCTION IN THE AMOUNT OF THE ACTUAL COST TO THE DEPARTMENT SHALL BE MADE FOR MATERIALS DAMAGED BY THE CONTRACTOR WHICH WERE NOT INSTALLED AND WERE NOT RETURNED TO THE DEPARTMENT.

RETURN OF NON-PERFORMED RAISED PAVEMENT MARKER MATERIALS SUPPLIED BY THE DEPARTMENT

RAISED PAVEMENT MARKER MATERIALS SUPPLIED BY THE DEPARTMENT, THAT ARE NON-PERFORMED, SHALL BE CAREFULLY PACKED OR REPACKED IN THE BOXES SUPPLIED BY THE RAISED PAVEMENT MARKER RECYCLER. BOXES SHALL BE MARKED WITH THE RECYCLER'S PART OR CATALOG NUMBER, THE ODOT PROJECT NUMBER, THE STYLE OF THE CASTING, AND THE COLOR OF THE PRISMATIC RETRO-REFLECTOR. THE RECYCLER'S CATALOG OR PART NUMBERS MAY BE OBTAINED FROM THE OFFICE OF TRAFFIC ENGINEERING IN COLUMBUS, OHIO. CASTING STYLES SHALL NOT BE MIXED WITHIN A BOX. ANY BOXES NOT PROPERLY PACKED OR MARKED WILL NOT BE ACCEPTED AT THE RECYCLER'S WAREHOUSE.

THE BOXES SHALL BE PLACED ON SKIDS OR PALLETS WITH ONLY ONE STYLE (LOW PROFILE OR CONVENTIONAL, REFLECTORIZED OR NON-REFLECTORIZED) AND NO MORE THAN 21 BOXES (420 RPM'S) ON EACH SKID.

NON-PERFORMED MATERIALS SHALL BE RETURNED, TO A LOCATION SPECIFIED BY THE DISTRICT ENGINEER, WITHIN 30 CALENDER DAYS OF THE COMPLETION OF THE PROJECT.

THE ABOVE WORK INCLUDING ALL LABOR, EQUIPMENT, AND MATERIAL NEEDED TO PERFORM THE WORK, SHALL BE CONSIDERED INCIDENTAL TO THE RESPECTIVE PAY ITEM.

IF THE DEPARTMENT HAS TO REPACKAGE THE RPMS CORRECTLY, THE CONTRACTOR WILL BE ASSESSED THE ACTUAL COST FOR REPACKAGING THE MATERIALS BY THE DEPARTMENT'S FORCES.

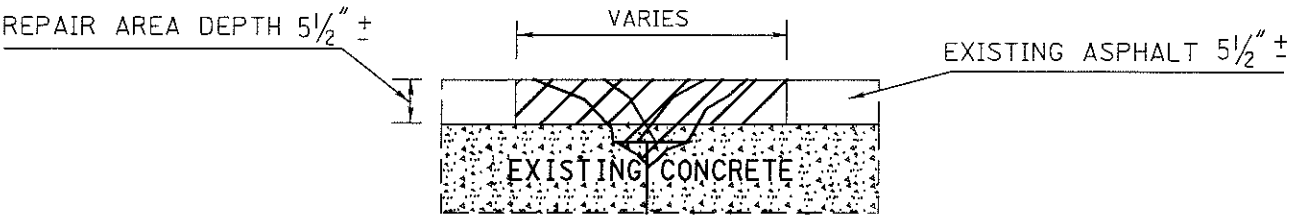
ITEM 25I PARTIAL DEPTH PAVEMENT REPAIR

THE CONTRACTOR SHALL PERFORM THE PARTIAL DEPTH PAVEMENT REPAIRS BY NOVEMBER 15, 2002

ITEM 25I PARTIAL DEPTH PAVEMENT REPAIR

ALL AREAS OF PARTIAL DEPTH PAVEMENT REPAIR SHALL BE COMPLETED AT THE END OF EACH DAY'S OPERATION AND OPEN TO NORMAL FLOW OF TRAFFIC. ITEM-30I BITUMINOUS AGGREGATE BASE PLACED IN PAVEMENT REMOVAL AREAS SHALL BE COMPACTED THOROUGHLY TO MINIMIZE SETTLEMENT OR DISPLACEMENT UNDER TRAFFIC. THE SURFACE OF THE PATCHES SHALL BE MAINTAINED FLUSH WITH THE EXISTING SURFACE UNTIL THE RESURFACING IS PLACED. THE FOLLOWING QUANTITY OF PARTIAL DEPTH PAVEMENT REPAIR HAS BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER.

AN ESTIMATED QUANTITY OF 200 CU. YDS. OF ITEM 25I-PARTIAL DEPTH PAVEMENT REPAIR.



EXISTING DETERIORATED ASPHALT AND ANY LOOSE CONCRETE SHALL BE REMOVED TO A DEPTH AS DIRECTED BY THE ENGINEER AND REPLACED WITH ITEM 30I BITUMINOUS AGGREGATE BASE. THE 30I SHALL BE COMPACTED AS PER 401.14 OF THE SPECIFICATIONS. THE BOTTOM AND VERTICAL FACE OF THE REPAIR AREAS SHALL BE PAINTED WITH 407 TACK COAT BEFORE THE 30I IS PLACED.

THE LOCATION AND SIZE OF THE REPAIRS WILL BE AS DIRECTED BY THE ENGINEER.

SPECIAL REQUIREMENTS

IF THE CONTRACTOR SCHEDULES ANY MILLING AND RESURFACING OPERATIONS IN CALENDER YEAR 2002, ALL PERMANENT PAVEMENT MARKINGS AND NEW RAISED PAVEMENT MARKERS MUST BE IN PLACE FOR ANY RESURFACED AREAS BEFORE WINTER. EXISTING PAVEMENT MARKINGS AND RAISED PAVEMENT MARKERS WHICH ARE REMOVED IN 2002 SHALL BE REPLACED WITH NEW REPLACEMENT PERMANENT PAVEMENT MARKINGS AND RAISED PAVEMENT MARKERS BEFORE NOVEMBER 15, 2002.

GENERAL NOTES

WAR-75-3.40

EXISTING PAVEMENT MARKINGS

IF DURING THE DURATION OF THE CONTRACT THE CONTRACTOR HAS TO ALTER THE PAVEMENT MARKINGS OUTSIDE THE PROJECT LIMITS, TO COMPLETE THE WORK WITHIN THE LIMITS, THEY SHALL BE REQUIRED TO REPLACE ALL SUCH MARKINGS WITH THE SAME MATERIAL OR IF THE STATE OR ANY OTHER MAINTAINING AGENCY IS CALLED TO SUSPEND PAINTING OPERATION DUE TO THE EXISTING CONSTRUCTION, THE CONTRACTOR SHALL BE REQUIRED TO REPAINT THE EXISTING PAVEMENT MARKINGS WHICH THE MAINTAINING AGENCY WAS UNABLE TO PAINT. PAYMENT OF THIS WORK SHALL BE INCLUDED UNDER ITEM 614 - MAINTAINING TRAFFIC.

THE PERMANENT TRAFFIC MARKINGS SHALL BE IN PLACE WITHIN TWENTY-ONE (21) CALENDAR DAYS OF THE COMPLETION OF THE PROJECT PER ODOT SPECIFICATIONS.

PERMANENT PAVEMENT MARKINGS

THE CONTRACTOR SHALL REFERENCE ALL PAVEMENT MARKINGS INCLUDING AUXILIARY PAVEMENT MARKINGS BEFORE THE START OF THE PLANING OPERATION. THIS WILL BE NECESSARY TO ASSURE CORRECT PLACEMENT OF MARKINGS IN ORIGINAL LOCATIONS. PAY FOR THIS OPERATION SHALL BE INCLUDED WITH EACH RESPECTIVE PAVEMENT MARKING ITEM.

INTERIM COMPLETION DATE

AN INTERIM DATE OF COMPLETION FOR THIS CONTRACT IS SET 30 DAYS PRIOR TO THE FINAL COMPLETION DATE. ALL CONTRACT ITEMS OF WORK MUST BE COMPLETED BY THE INTERIM COMPLETION DATE. THE INTERIM DATE WILL BE SUBJECT TO LIQUIDATED DAMAGES AS INDICATED BY SECTION 108.07 OF THE CONSTRUCTION AND MATERIALS SPECIFICATIONS BOOK. REQUEST FOR TIME EXTENSIONS TO THE INTERIM COMPLETION DATE WILL BE PROCESSED AS PER SECTION 108.06 OF THE SPECIFICATIONS BOOK. THE PERIOD OF TIME BETWEEN THE INTERIM COMPLETION DATE AND THE FINAL COMPLETION DATE IS STRICTLY TO ALLOW FOR COMPLETION OF THE "PUNCH LIST" ITEMS AND REMOVAL OF THE PROJECT FIELD OFFICES. FAILURE TO COMPLETE THE "PUNCH LIST" AND REMOVE THE FIELD OFFICES BY THE FINAL COMPLETION DATE WILL RESULT IN ASSESSMENT OF LIQUIDATED DAMAGES AS PER SECTION 108.07 OF THE SPECIFICATIONS BOOK.

A GRANTED TIME EXTENSION TO THE INTERIM COMPLETION DATE WILL NOT INCLUDE A CORRESPONDING EXTENSION TO THE FINAL COMPLETION DATE. EXTENSIONS OF TIME TO THE FINAL COMPLETION DATE WILL ONLY BE GRANTED IF IT CAN BE JUSTIFIED THAT NOT ENOUGH TIME EXIST TO COMPLETE "PUNCH LIST" ITEMS AND REMOVE THE PROJECT FIELD OFFICES PRIOR TO THE FINAL COMPLETION DATE.

ITEM 618 - RUMBLE STRIPS, TYPE 2

THE FOLLOWING QUANTITIES OF RUMBLE STRIPS, TYPE 2 HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

NORTHBOUND LEFT SHOULDER - 8.80 MILES
NORTHBOUND RIGHT SHOULDER - 8.80 MILES
SOUTHBOUND LEFT SHOULDER - 8.80 MILES
SOUTHBOUND RIGHT SHOULDER - 8.80 MILES

TOTAL RUMBLE STRIPS, TYPE 2 = 35.20 MILES

TEMPORARY PAVEMENT MARKINGS

PRIOR TO OPENING THE MAINLINE AND RAMP SECTIONS TO NORMAL TRAFFIC FLOW, THE CONTRACTOR SHALL HAVE COMPLETED THE INSTALLATION OF ALL PAVEMENT MARKING PERTINENT TO THE SAFE OPERATION OF THE TRAFFIC. IF PERMANENT PAVEMENT MARKINGS CAN NOT BE PLACED AT THIS TIME, TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED.

THE FOLLOWING QUANTITIES OF TEMPORARY PAVEMENT MARKINGS HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR THE ABOVE PURPOSE:

ITEM	QUANT.	UNIT	DESCRIPTION
614	20.81	MILE	TEMPORARY EDGE LINE, CLASS 1 (YELLOW)
614	20.81	MILE	TEMPORARY EDGE LINE, CLASS 1 (WHITE)
614	35.87	MILE	TEMPORARY LANE LINE, CLASS 1
614	5871	LN.FT.	TEMPORARY CHANNELIZING LINE, CLASS 1
614	2401	LN.FT.	TEMPORARY TRANSVERSE LINE, CLASS 1
614	156	LN.FT.	TEMPORARY STOP LINE, CLASS 1
614	20	EACH	TEMPORARY LANE ARROW, CLASS 1
614	8	EACH	TEMPORARY WORD ON PAVEMENT "ONLY", CLASS 1

DETECTOR LOOP

PRIOR TO THE PLANING OPERATION, THE LOCATIONS OF THE EXISTING LOOP DETECTORS SHALL BE REFERENCED SO THAT THE REPLACEMENT LOOP CAN BE REINSTALLED AT THE PROPER LOCATION. THE NEW LOOP DETECTORS SHALL BE CONNECTED TO THE LEAD-IN CABLE WITH THE PROPER CONNECT KITS AND TESTED TO MAKE CERTAIN THAT THEY ARE OPERATIONAL IN A FEW SITUATIONS. THE LOOP DETECTOR LOCATIONS MAY NEED TO BE MODIFIED. THE CONTRACTOR SHALL NOTIFY THE DISTRICT 8 TRAFFIC DEPARTMENT A MINIMUM OF 3 DAYS IN ADVANCE OF ANY LOOP DETECTOR INSTALLATIONS TO PERMIT TIME FOR LOOP LOCATION ADJUSTMENTS IF NEEDED.

THE FOLLOWING QUANTITIES OF DETECTOR LOOP AND LOOP DETECTOR TIE IN HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

LOCATION:

RAMP "M" @ SR-123 = 2 EACH
RAMP "D" @ SR-123 = 2 EACH
RAMP "D" @ SR-73 = 2 EACH
RAMP "K" @ SR-73 = 2 EACH

ITEM 632 - LOOP DETECTOR TIE IN - 8 EACH
ITEM 632 - DETECTOR LOOP - 8 EACH

GENERAL REQUIREMENTS

IT IS THE INTENTION TO PERFORM THE WORK REQUIRED WITH THE LEAST INCONVENIENCE TO, AND THE MAXIMUM SAFETY OF THE CONTRACTOR AND THE TRAVELING PUBLIC. ANY VARIANCES FROM THESE MAINTENANCE OF TRAFFIC NOTES MUST BE APPROVED IN ADVANCE IN WRITING BY THE DIRECTOR. THE REQUIREMENTS FOR MAINTAINING TRAFFIC AS INDICATED IN THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, CURRENT EDITION, AND PERTINENT ITEMS OF THE SPECIFICATIONS AND PROPOSAL SHALL APPLY.

TRAFFIC IS TO BE MAINTAINED IN A UNIFORM PATTERN THROUGHOUT THE ENTIRE LENGTH OF THE PROJECT AND NOT TO BE SUBJECTED TO CONSTANT LANE SHIFTS.

WORK CAN BE PERFORMED SIMULTANEOUSLY IN THE NORTHBOUND AND SOUTHBOUND LANES.

ALL VEHICLES, EQUIPMENT, PERSONS, AND ACTIVITIES ARE RESTRICTED AT ALL TIMES TO ONE SIDE OF THE DIRECTIONAL PAVEMENT LANES UNLESS OTHERWISE APPROVED BY THE ENGINEER. VEHICLES AND EQUIPMENT SHALL ALWAYS MOVE WITH, AND NOT ACROSS OR AGAINST THE FLOW OF TRAFFIC.

VEHICLES AND EQUIPMENT SHALL NOT PARK OR STOP EXCEPT WITHIN DESIGNATED WORK AREAS, AND SHALL ENTER AND LEAVE WORK AREAS IN A MANNER WHICH WILL NOT BE HAZARDOUS TO, OR INTERFERE WITH THE NORMAL FLOW OF TRAFFIC. PERSONAL VEHICLES WILL NOT BE PERMITTED TO PARK WITHIN THE RIGHT-OF-WAY EXCEPT IN SPECIFIC AREAS DESIGNATED BY THE ENGINEER. EQUIPMENT, VEHICLES AND MATERIAL SHALL NOT BE STORED OR PARKED ON THE PROJECT WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED PAVEMENT UNLESS THEY ARE 6 FT. BEHIND GUARDRAIL.

THE CONTRACTOR WILL BE REQUIRED TO PROVIDE, ERECT, MAINTAIN (IN PROPER POSITION, KEEP CLEAN, LEGIBLE AND IN GOOD WORKING CONDITION), AND REMOVE LIGHTS, SIGNS, AND OTHER TRAFFIC CONTROL DEVICES NECESSARY FOR THE MAINTENANCE OF TRAFFIC, INCLUDING REGULATORY SIGNS AND PAVEMENT MARKINGS. ALL SIGNS SHALL BE POST MOUNTED.

PLACEMENT OF ALL TRAFFIC CONTROL DEVICES SHALL PROCEED IN THE DIRECTION OF THE FLOW OF TRAFFIC. THE CONTRACTOR SHALL INSTALL ALL NECESSARY TRAFFIC CONTROL DEVICES BEFORE BEGINNING WORK AND SHALL REMOVE THEM IMMEDIATELY UPON COMPLETION OR SUSPENSION OF WORK.

MAINLINE CLOSURES SHALL BE LIMITED TO A MAXIMUM OF 2 MILES IN LENGTH AT ANY ONE TIME.

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. THE INSTALLATION AND OPERATION OF ALL TRAFFIC CONTROL AND TRAFFIC CONTROL DEVICES SHALL BE PROVIDED BY THE CONTRACTOR WHETHER INSIDE OR OUTSIDE THE WORK LIMITS.

SPECIFIC REQUIREMENTS

THE MAINLINE AND RAMPS FOR I-75 SHALL BE OPEN TO TRAFFIC WITHOUT RESTRICTION DURING THE FOLLOWING WEEKEND HOLIDAYS: MEMORIAL DAY, FOURTH OF JULY, LABOR DAY AND THANKSGIVING. A "WEEKEND", FOR TRAFFIC MAINTENANCE PURPOSES, IS DEFINED AS FOLLOWS, WHEN A NATIONAL HOLIDAY IS ON:

- A) MONDAY OR TUESDAY -- FROM 5:30 A.M. ON THE FRIDAY BEFORE UNTIL 9:00 P.M. ON THE DAY FOLLOWING.
- B) WEDNESDAY -- FROM 5:30 A.M. ON THE DAY BEFORE UNTIL 9:00 P.M. ON THE DAY FOLLOWING.
- C) THURSDAY OR FRIDAY -- FROM 5:30 A.M. ON THE DAY BEFORE UNTIL 9:00 P.M. ON THE MONDAY FOLLOWING.
- D) SATURDAY OR SUNDAY -- FROM 5:30 A.M. ON THE FRIDAY BEFORE UNTIL 9:00 P.M. ON THE MONDAY FOLLOWING.

ALL MAINLINE, SHOULDER AND RAMP WORK SHALL BE PERFORMED DURING "NIGHTTIME WORK HOURS." AT THE END OF EACH WORK SHIFT, THE OVERLAY SURFACE COURSE SHALL BE COMPLETE, THE TEMPORARY OR PERMANENT PAVEMENT MARKINGS SHALL BE INSTALLED, AND I-75 AND RAMPS SHALL BE OPENED TO THE EXISTING NUMBER OF LANES OF TRAFFIC IN EACH DIRECTION DURING THE DAYTIME HOURS. FAILURE TO OPEN THREE (3) PROPERLY MARKED LANES OF TRAFFIC IN EACH DIRECTION DURING THE DAYTIME HOURS SHALL SUBJECT THE CONTRACTOR TO THE FOLLOWING LIQUIDATED DAMAGES:
\$1,000/HR FOR A SINGLE LANE CLOSURE
\$5,000/HR FOR A TWO LANE CLOSURE

"NIGHTTIME WORK HOURS" - SINGLE LANE CLOSURES - 9:00 P.M. TO 6:00 A.M.
"NIGHTTIME WORK HOURS" - TWO LANE CLOSURES - 10:00 P.M. TO 5:30 A.M.

MAINTENANCE PERSON FOR TRAFFIC CONTROL DEVICES

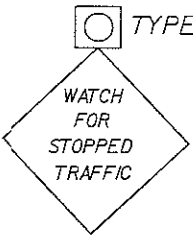
IN ORDER TO PROVIDE A SAFE FACILITY FOR THE TRAVELING PUBLIC THE CONTRACTOR SHALL PROVIDE THE FOLLOWING:

- 1) DURING ALL HOURS WHEN TRAFFIC IS RESTRICTED TO LESS THAN THE NUMBER OF EXISTING LANES IN ANY DIRECTION, THE CONTRACTOR SHALL EMPLOY AT LEAST ONE QUALIFIED PERSON FOR EACH SHIFT TO CONTINUOUSLY PATROL THE PROJECT AREA TO MAINTAIN ALL LIGHTS, DRUMS, SIGNS, BARRICADES, ECT. HE SHALL HAVE AVAILABLE AT ALL TIMES THE TOOLS AND MATERIALS NECESSARY TO PERFORM THIS FUNCTION. THE NAMES AND TELEPHONE NUMBERS OF THESE PERSONS SHALL BE SUPPLIED TO THE ENGINEER.
- 2) DURING ALL PERIODS NOT COVERED BY ITEM 1 ABOVE, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER THE NAMES AND TELEPHONE NUMBERS OF A PERSON OR PERSONS WHO CAN BE CONTACTED 24 HOURS PER DAY BY THE OHIO DEPARTMENT OF TRANSPORTATION AND ALL INTERESTED POLICE AGENCIES. THIS PERSON OR PERSONS SHALL ALSO BE RESPONSIBLE FOR MAINTAINING ALL TRAFFIC CONTROL DEVICES IN WORKING ORDER. THE CONTRACTOR SHALL PROVIDE THE NAME AND TELEPHONE NUMBER FOR THE CONTACT PERSON FOR EACH SHIFT.

ADVANCE WARNING SIGNS

ADVANCE WARNING SIGNS SHALL BE INSTALLED ON MAINLINE I-275 AS PER THE "OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" AND STANDARD DRAWING MT-95.30M DATED 04/25/94.

ALSO "ROAD CONSTRUCTION NEXT MILES" (OC-6) SIGNS AND "END CONSTRUCTION" (OC-8) SIGNS SHALL BE INSTALLED ON I-275 AT THE PROJECT LIMITS. THERE SHALL BE AVAILABLE ON THE JOB AT ALL TIMES SIX (6) SPECIAL BLACK ON ORANGE "WATCH FOR STOPPED TRAFFIC" SIGNS (48" X 48"). THERE SHALL BE TWO FOR EACH DIRECTION OF TRAFFIC AND TWO SPARES. THESE SIGNS SHALL BE MOUNTED ON PORTABLE BARRICADES, AND ARE TO BE USED IN THE EVENT THAT TRAFFIC BACKUPS OCCUR. THEY WILL BE LOCATED APPROXIMATELY 1/4 MILE IN ADVANCE OF THE BACKUP AND WILL BE MOVED BACK AS THE BACK UP INCREASES.



TYPE B WARNING LIGHT

OW-227B 48 SPECIAL
7 LETTER SIZE
C SERIES
12 VOLT AMBER FLASHER

IN ADDITION TO MAINLINE, THE CONTRACTOR SHALL INSTALL "ROAD CONSTRUCTION AHEAD" (OW-128) SIGNS ON ALL RAMPS LEADING TO I-275.

ITEM 614 - MAINTAINING TRAFFIC

ALL THE WORK DESCRIBED IN THE "MAINTENANCE OF TRAFFIC NOTES" SHALL BE INCLUDED FOR PAYMENT UNDER ITEM 614 MAINTAINING TRAFFIC EXCEPT FOR THE ITEMS WHICH ARE TABULATED SEPARATELY IN THE GENERAL SUMMARY.

CHECKED	CALCULATED
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Detail	TC-65.12
8	ONE LANE BRIDGE
9	STOP APPROACH
10	TWO WAY LEFT TURN
11	HORIZONTAL CURVE
12	APPROACH W/ LEFT TURN LANE

RAISED PAVEMENT MARKER

WAR-75-3.40

$$\frac{11}{16}$$

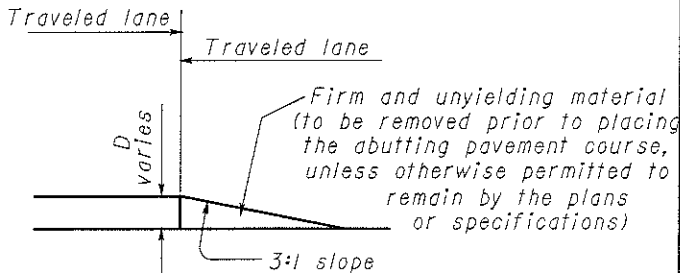
AN ESTIMATED QUANTITY OF 1662 EA. ITEM 202 RAISED PAVEMENT MARKERS REMOVED FOR STORAGE, AS PER PLAN, HAS BEEN PROVIDED. THE CONTRACTOR SHALL REMOVE ALL RAISED PAVEMENT MARKERS WITHIN THE LIMITS OF WORK AND DELIVER THEM TO THE OHIO DEPARTMENT OF TRANSPORTATION, DISTRICT 8, LEBANON, OHIO, IN A RELATIVELY CLEAN CONDITION (NO MUD OR DEBRIS MIXED IN). THE DEPRESSIONS LEFT BY THEIR REMOVAL SHALL BE TACKED WITH ITEM 407 MATERIAL AND THEN ITEM 448 MATERIAL SHALL BE PLACED AND COMPACTED. THE COST FOR THIS ITEM SHALL INCLUDE ALL OF THE MATERIALS, EQUIPMENT AND LABOR DESCRIBED ABOVE. EXISTING RAISED PAVEMENT MARKERS SHALL NOT BE REMOVED UNLESS THEY WILL BE REPLACED IN THE SAME CONSTRUCTION SEASON.

GENERAL NOTES

1. It is intended that this drawing be used for treatment of drop-offs that develop during construction operations, and that are not otherwise provided for in the construction plans. Where the plans do not provide specific items for labor, equipment, or materials to implement the drop-off treatments specified hereon, they shall be included for payment in the lump sum bid for Item 614 - Maintaining Traffic.
2. While the need for certain advisory signing is noted hereon, it is not intended that this be indicative of all signing that may be required to advise or warn motorists, and all requirements of the Ohio Manual of Uniform Traffic Control Devices (OMUTCD) must be fulfilled.
3. In urban or otherwise heavily developed areas where pedestrians and/or bicyclists may be present in significant numbers, additional signing and protective measures other than those shown hereon may be required.
4. The drop-off treatment selected for use at any given location shall be as appropriate for the prevailing conditions at the site.
5. Where concrete barrier is specified, it shall be in accordance with Standard Construction Drawing MC-9.2 and Item 622.
6. When drums are specified for a dropoff condition, a minimum number of four drums shall be used. Spacing shall be as indicated in the plans or as specified in the OMUTCD.
7. When OW-151 (Low Shoulder) signs or OW-171 (Uneven Lanes) and OWP-171 signs are required, they shall be placed 750' in advance of the condition, on all intersecting entrance ramps within the limits of the condition and immediately beyond all intersecting roadways within the limits of the condition. When the dropoff condition extends more than one-half mile, additional signs should be erected at intervals of one mile or less.
8. For locations, such as at ramps, lane shifts, lane closures, etc., where traffic is required to negotiate any difference in elevation between pavements, a 3:1 slope treatment similar to the Optional Wedge Treatment shall be provided.
9. Portable concrete barrier shall be placed on the same level as the traffic surface and shall not encroach on lane width(s) designated as the minimum required for traffic use. Where drums are used, and their presence would reduce traveled lane widths to less than 10', drums may be placed on the opposite level from that of traffic provided the dropoff depth does not exceed 5" and approval is granted by the Project Engineer.
10. Pavement Repairs (or similar work):
- a. Lengths greater than 60 feet - utilize appropriate treatment from Condition I.
- b. Lengths of 60 feet or less - repairs shall be effected in accordance with 255.08. Drums may be used as a separator adjacent to the traveled lane.

OPTIONAL WEDGE TREATMENT
(MILLING OR RESURFACING)

1. This treatment may be used when permitted for Condition I only.
2. OW-171 and OWP-171 signs required.



CONDITION I
DROPOFFS BETWEEN TRAVELED LANES

1. These treatments are to be used for resurfacing, pavement planing, excavation, etc. between or within traveled lanes.

D (In.)	Treatment
≤1½	Erect OW-171 and OWP-171 signs.
>1½-3	1) Lane closure utilizing drums* as shown below OR 2) Optional Wedge Treatment
>3-5	Lane closure utilizing drums as shown below.
>5	Lane closure utilizing portable concrete barrier as shown below.

*Cones may be used for daytime only conditions.

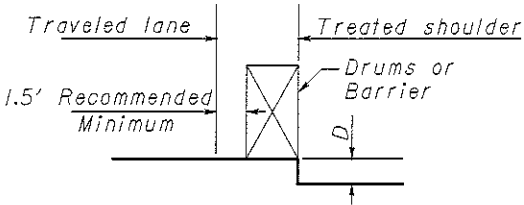


CONDITION II
DROPOFFS WITHIN GRADED SHOULDER AREA

1. The treatments indicated below are for use in conjunction with resurfacing, planing, or excavations within the graded shoulder area.
2. The graded shoulder area is that flat or gradually sloping area between the edge of a normally traveled lane and the more steeply sloping ditch foreslope or embankment slope. Its surface may be soil or turf, and/or it may be inclusive of a "treated" area (improved with aggregates, asphaltic materials, or concrete). For the purposes herein, its maximum width shall be considered to be twelve (12) feet.

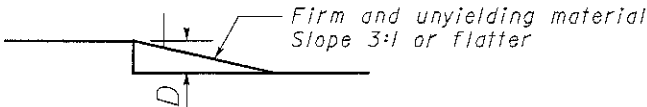
D (In.)	Treatment
≤1½	1) If edgelines are present, no treatment necessary OR 2) Erect OW-171 and OWP-171 signs.
>1½-5	1) If min. lane width requirements can be met, maintain lanes utilizing drums as shown below OR 2) If min. lane width requirements cannot be met, close adjacent lane utilizing drums OR 3) Optional Shoulder Treatment.
>5-12 Daylight only	If min. lane width requirements can be met, maintain lanes utilizing drums as shown below.
>5-24	1) If min. lane width requirements can be met, maintain lanes utilizing portable concrete barrier as shown below. OR 2) If min. lane width requirements cannot be met, close adjacent lane utilizing drums.
>24	Lane closure utilizing portable concrete barrier as shown below.

*Minimum lane widths shall be 10' unless otherwise specified in the plans.



OPTIONAL SHOULDER TREATMENT

1. This treatment may not be used within a bituminous shoulder where a hot longitudinal joint per 401.15 is required.
2. OW-151 signs required.



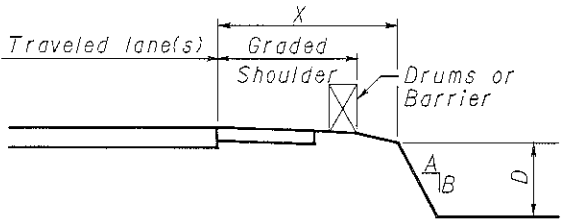
CONDITION III

DROPOFFS BEYOND GRADED SHOULDER OR BACK OF CURB

1. See Note 2 under Condition II.
2. Use Chart A or B below, as applicable.

CHART A

- USE FOR:
1. Uncurbed Facilities.
2. Curbed Facilities, where:
- a. Curbs are less than 6" in height.
- b. Curbs are 6" or greater in height and the legal speed is greater than 40 mph.

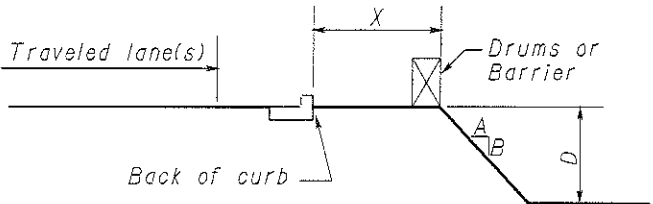


X (Ft.)	D (In.)	A/B	Treatment Required	
			Day	Night
2-4	Any	Any	(a)	(a)
4-30	Any	3:1 or Flatter	None	None
4-12	<3	Steeper than 3:1	None	None
4-12	>3-≤12	Steeper than 3:1	Drums	Drums
4-12	>12	Steeper than 3:1	Drums	Barrier
>12-20	<12	Steeper than 3:1	None	None
>12-20	>12-≤24	Steeper than 3:1	Drums	Drums
>12-20	>24	Steeper than 3:1	Drums	Barrier
>20-30	<24	Steeper than 3:1	None	Drums
>20-30	>24	Steeper than 3:1	Drums	Barrier
>30	Any	Any	None	None

(a) Use treatment specified under Condition II.

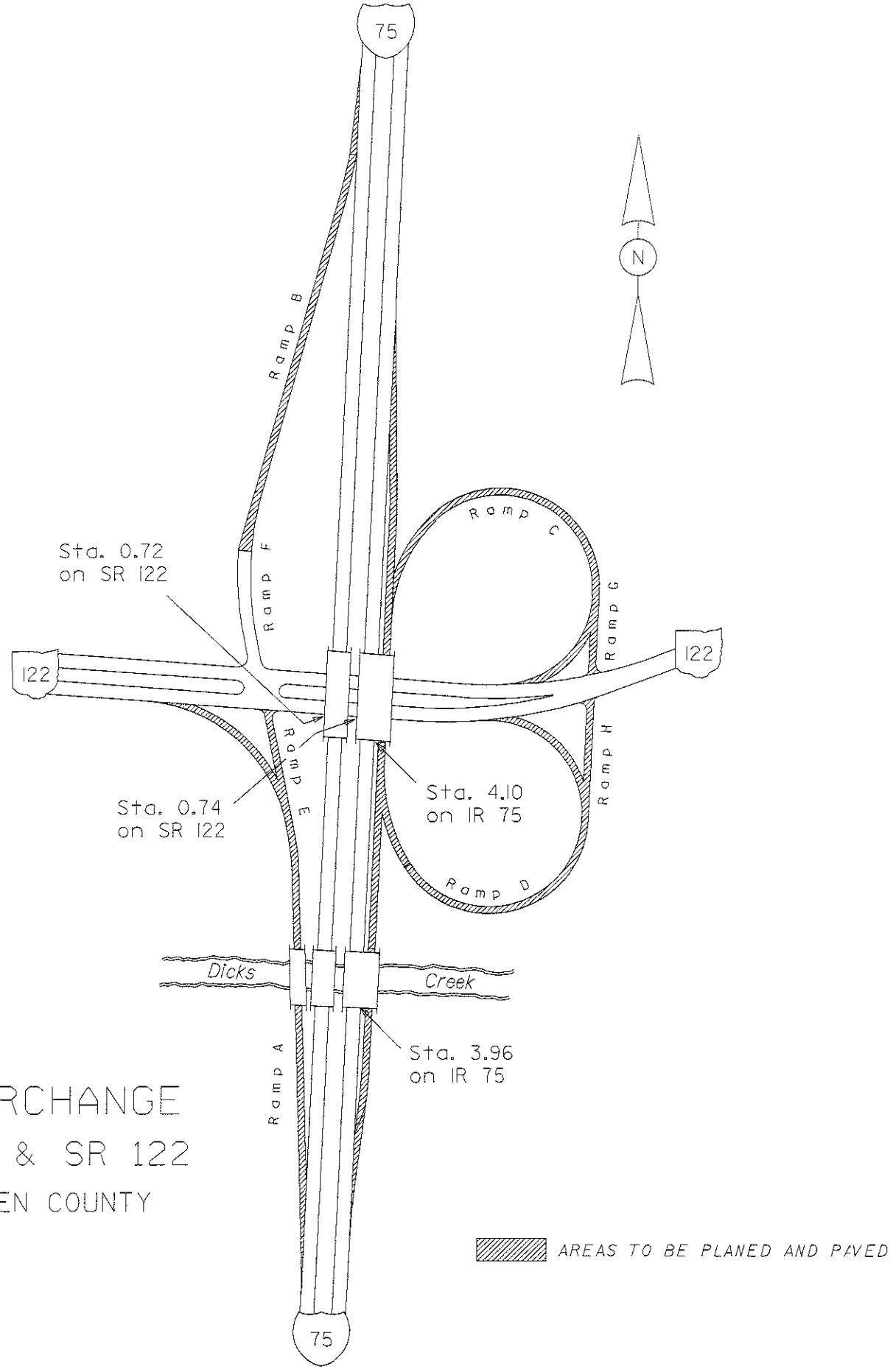
CHART B

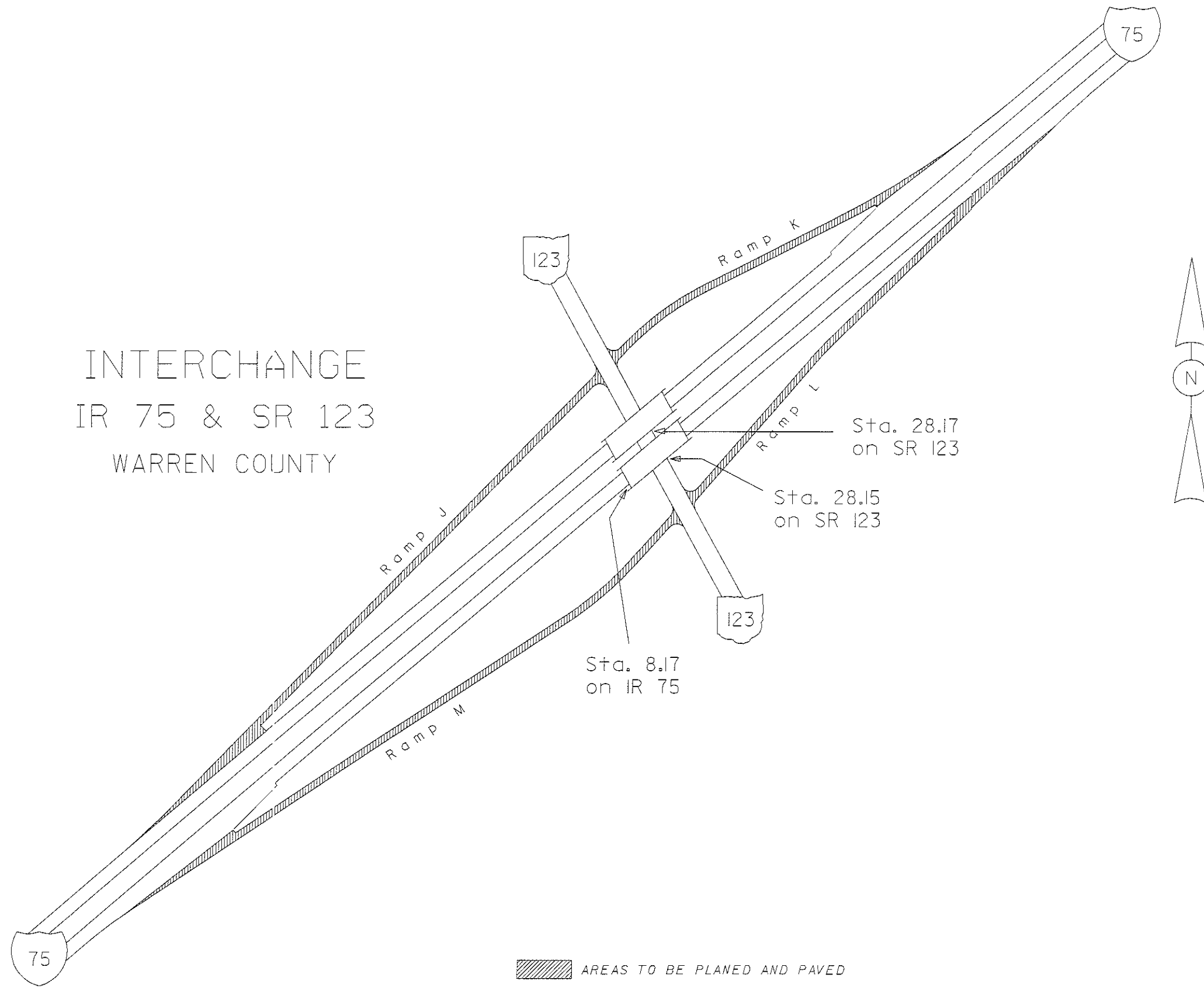
- USE FOR: Curbed facilities, where the curb is 6" or greater in height and the legal speed is 40 mph or less.

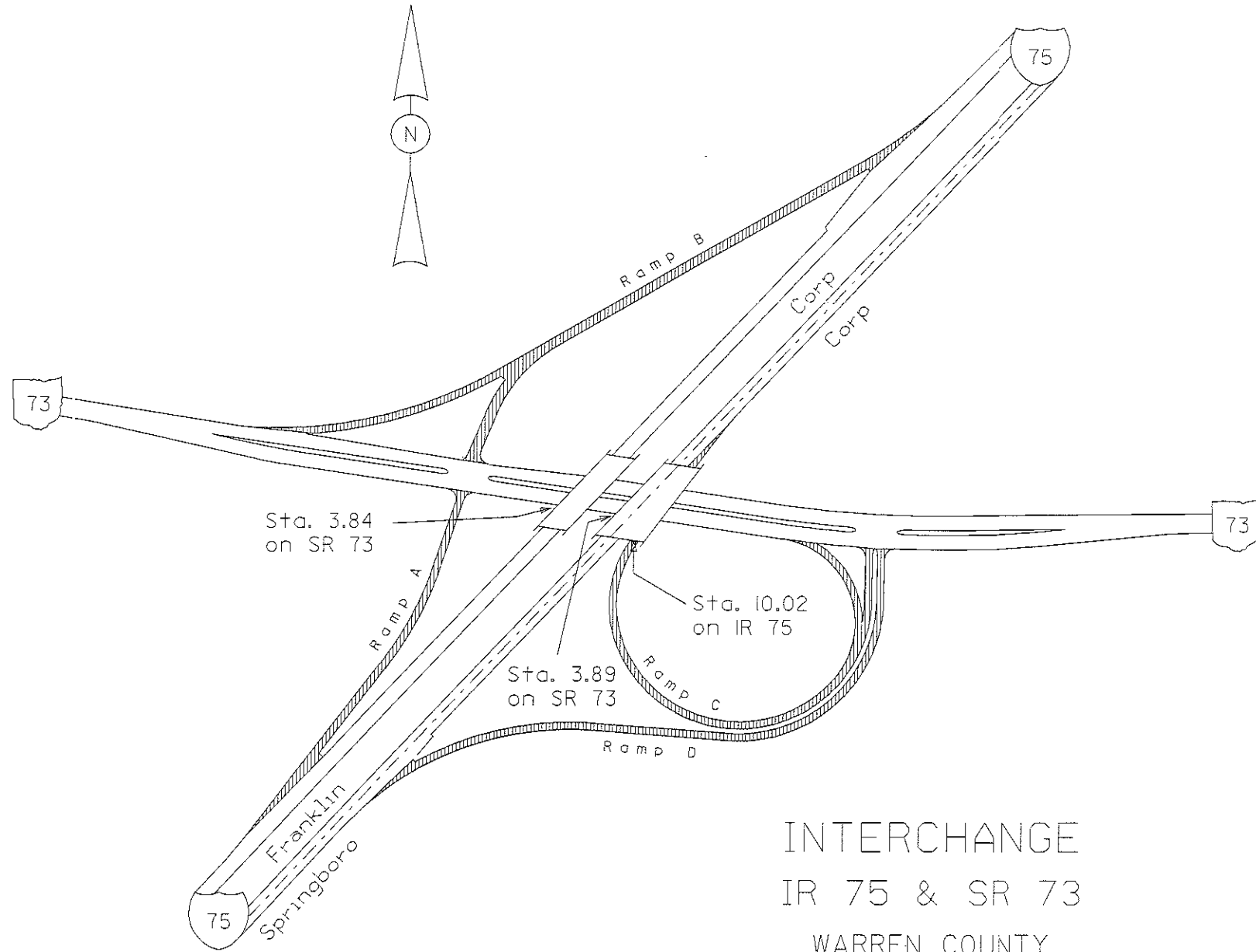



X (Ft.)	D (In.)	A/B	Treatment Required	
			Day	Night
0-10	<12	Any	None	Drums
0-10	>12	Any	Drums	Drums
>10	Any	Any	None	None

INTERCHANGE
IR 75 & SR 122
WARREN COUNTY







 AREAS TO BE PLANED AND PAVED

CALCULATED
 CHECKED

RAMP LOCATIONS

WAR-75-3.40

15
 6

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